

Town of Nottingham, New Hampshire

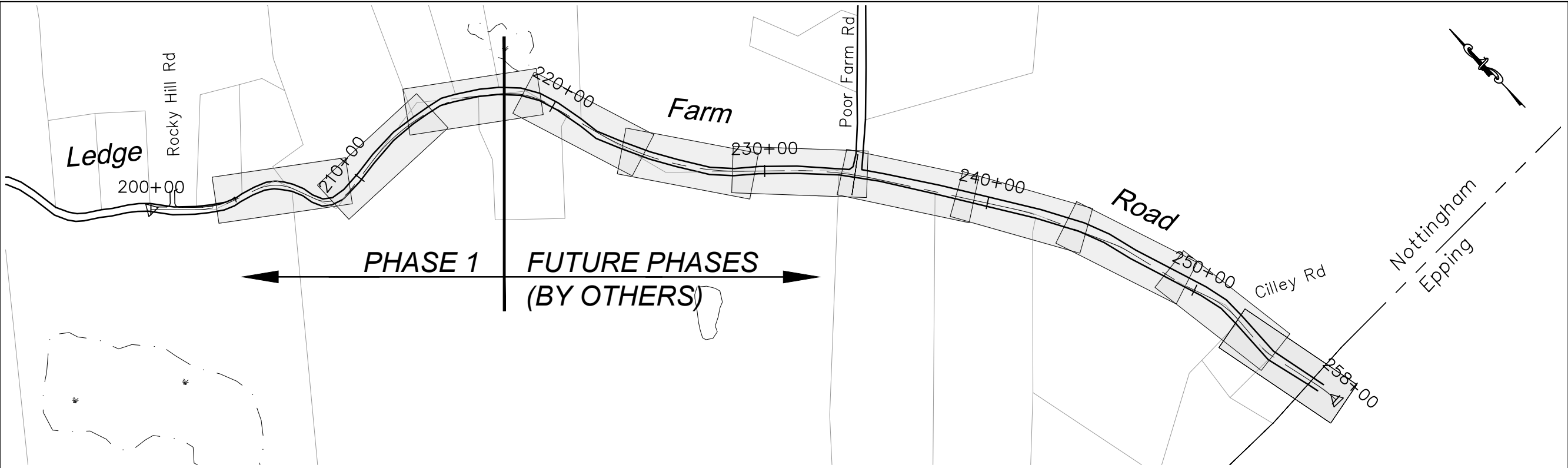
Highway Department

Ledge Farm Road Improvements

Issued for Bidding - July 2019

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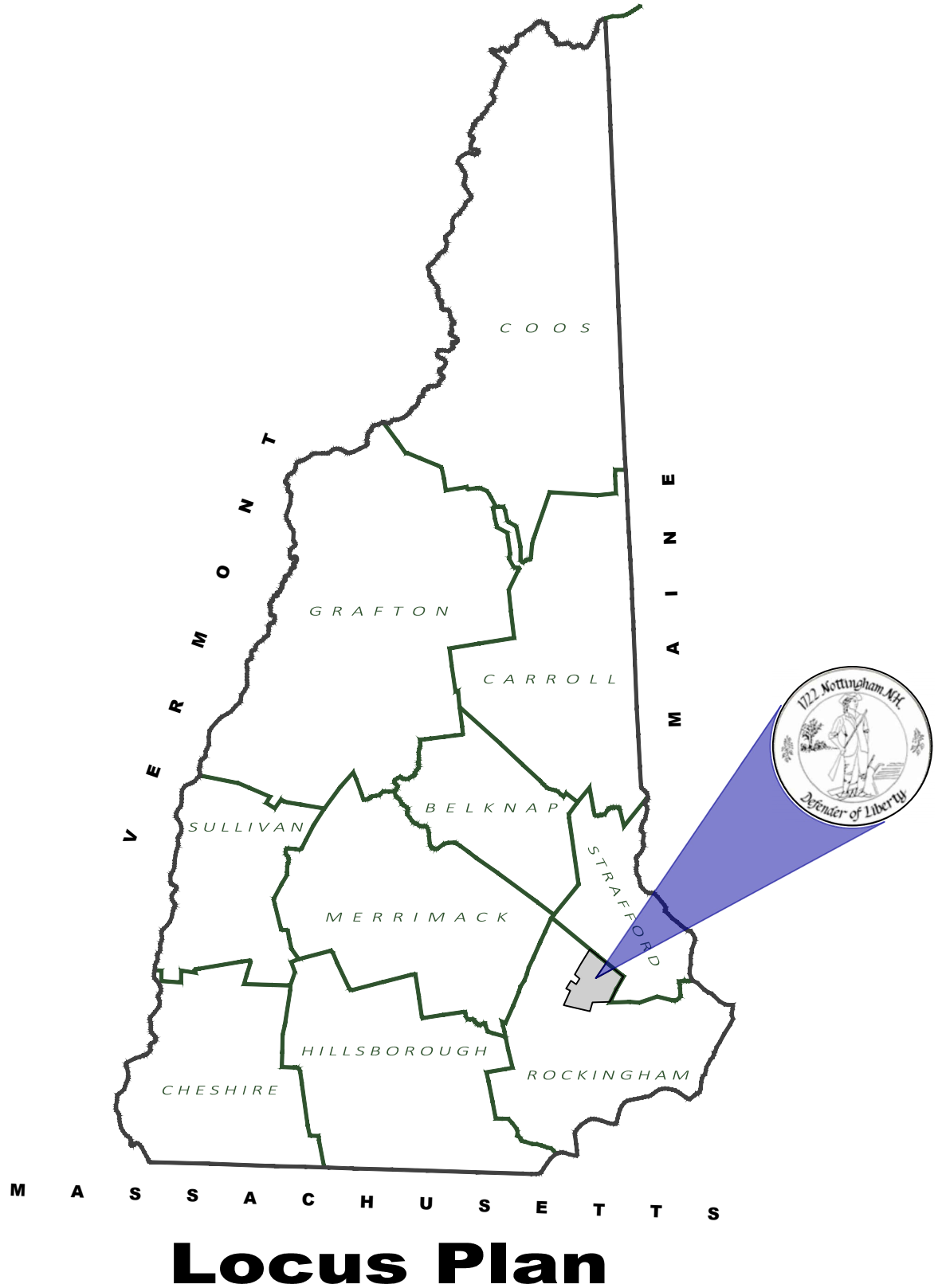


Key Plan:

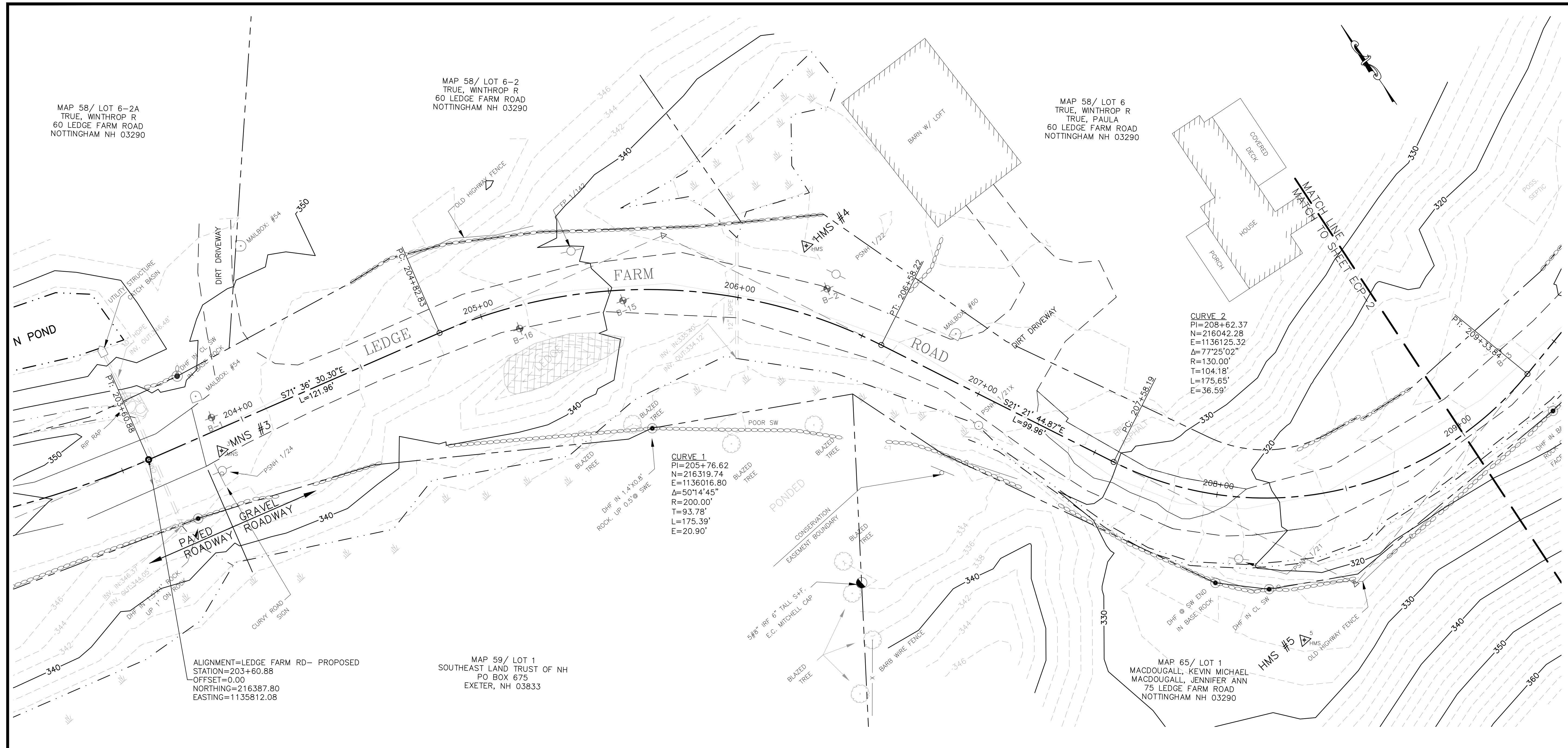
Prepared For:
Town of Nottingham
139 Stage Road
Nottingham, New Hampshire 03290
 Prepared By:

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 CONSTRUCTION



Project Location



MAP 58/ LOT 6-2A
TRUE, WINTHROP R
60 LEDGE FARM ROAD
NOTTINGHAM NH 03290

MAP 58/ LOT 6-2
TRUE, WINTHROP R
60 LEDGE FARM ROAD
NOTTINGHAM NH 03290

MAP 58/ LOT 6
TRUE, WINTHROP R
TRUE, PAULA
60 LEDGE FARM ROAD
NOTTINGHAM NH 03290

ALIGNMENT=LEDGE FARM RD- PROPOSED
STATION=203+60.88
OFFSET=0.00
NORTHING=216387.80
EASTING=1135812.08

MAP 59/ LOT 1
SOUTHEAST LAND TRUST OF NH
PO BOX 675
EXETER, NH 03833

MAP 65/ LOT 1
MACDOUGALL, KEVIN MICHAEL
MACDOUGALL, JENNIFER ANN
75 LEDGE FARM ROAD
NOTTINGHAM NH 03290

BENCHMARK TABLE (BM ##)

POINT #	NORTHING	EASTING	ELEV.	DESCRIPTION
208	216521.9365	1135588.6666	350.19	TBM+

CONTROL POINT TABLE (CP ##)

POINT #	NORTHING	EASTING	DESCRIPTION
2	216550.0868	1135624.1188	MNS
3	216371.6349	1135835.2185	MNS
4	216279.9878	1136055.0084	HMS
5	216039.4466	1136096.1960	HMS
6	216116.5752	1136331.5066	HMS
7	216063.3032	1136768.3468	SPIKE
8	215873.6044	1137049.5089	HMS
9	215547.6649	1137179.3461	SPIKE
10	215233.8817	1137277.0547	SPIKE
11	214953.9853	1137472.0560	SPIKE
12	214714.0897	1137760.5006	SPIKE
14	214078.0939	1138216.2029	SPIKE
15	213753.8700	1138409.8196	HMS
17	213029.5380	1138633.5219	SPIKE
91	212642.9467	1138673.0272	AVG_GPS2

no.	1	revision	Issued for Bidding	date	07/19	by	JWB
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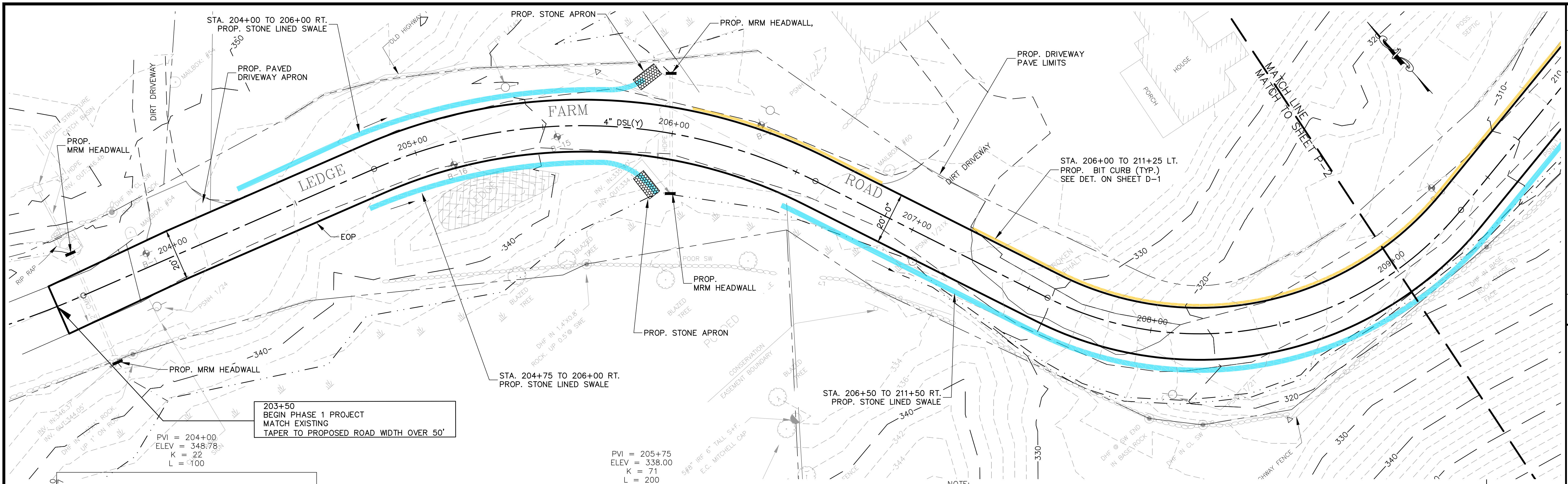
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date: July 2019
project no: 887
file name: 887_LFR EXIST COND PLANS.dwg WAS
designed by: JWB
drawn by: AGL
approved by: WMS
scale: 1" = 20'
0 20' 40'

Town of Nottingham, New Hampshire
Highway Department
Ledge Farm Road
Improvements
Existing Conditions Plan
Sta. 203+50 to Sta. 209+00

drawing no. **ECP-1**
sheet: --- of ---



203+50
BEGIN PHASE 1 PROJECT
MATCH EXISTING
TAPER TO PROPOSED ROAD WIDTH OVER 50'

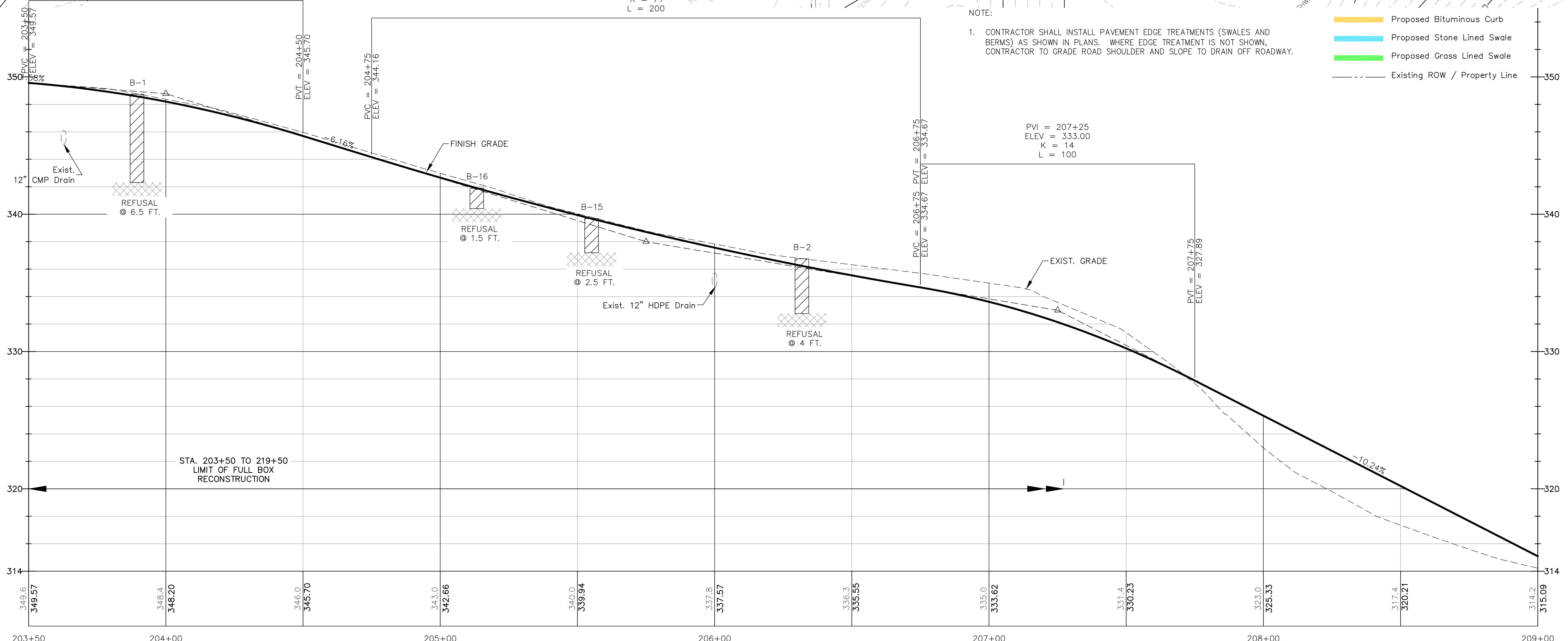
PVI = 204+00
ELEV = 348.78
K = 22
L = 100

PVI = 205+75
ELEV = 338.00
K = 71
L = 200

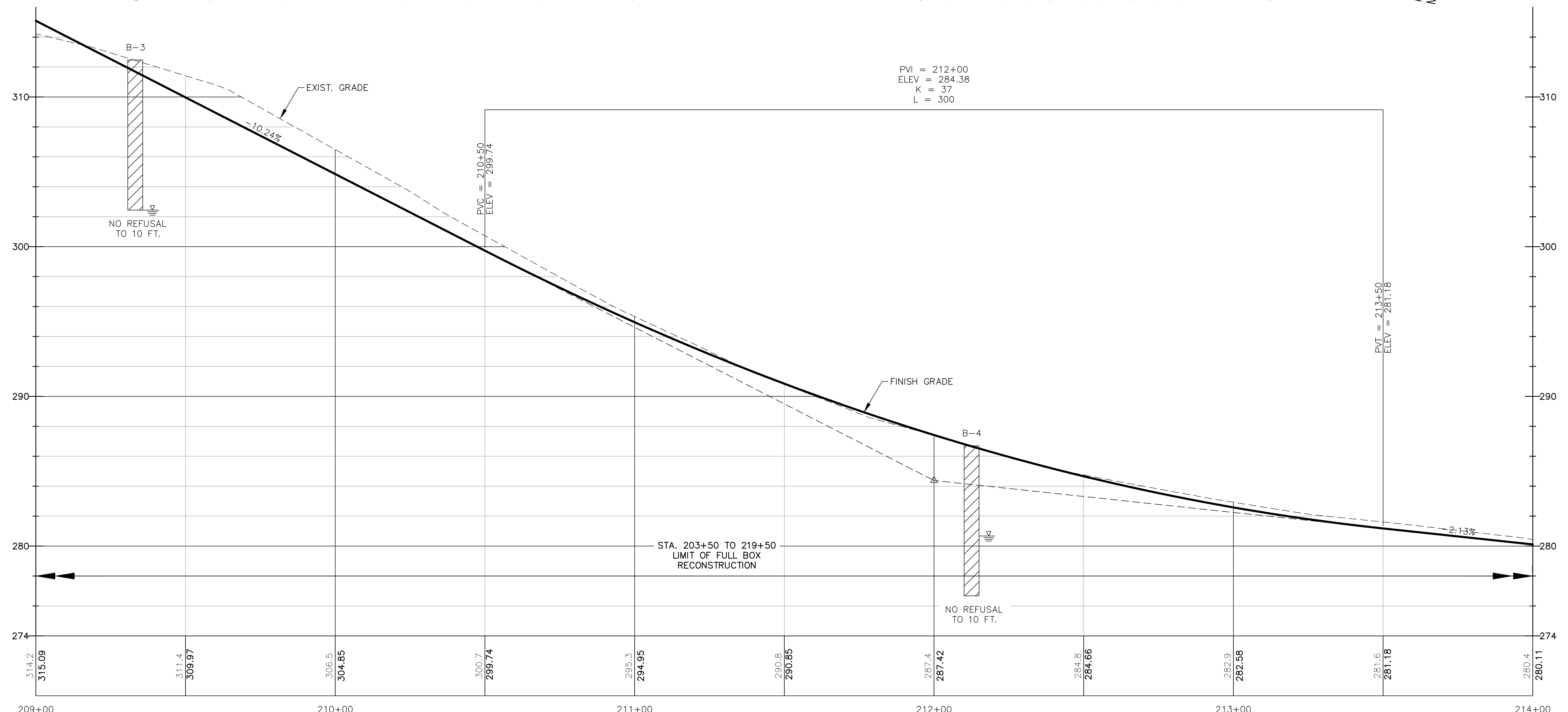
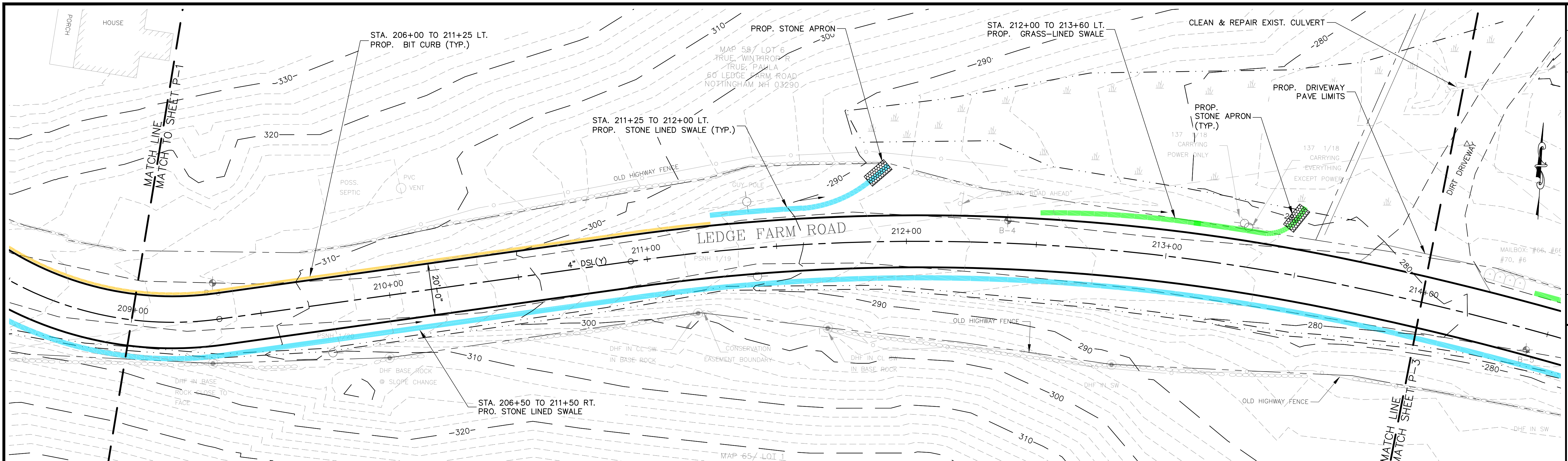
PVI = 207+25
ELEV = 333.00
K = 14
L = 100

NOTE:
1. CONTRACTOR SHALL INSTALL PAVEMENT EDGE TREATMENTS (SWALES AND BERMS) AS SHOWN IN PLANS. WHERE EDGE TREATMENT IS NOT SHOWN, CONTRACTOR TO GRADE ROAD SHOULDER AND SLOPE TO DRAIN OFF ROADWAY.

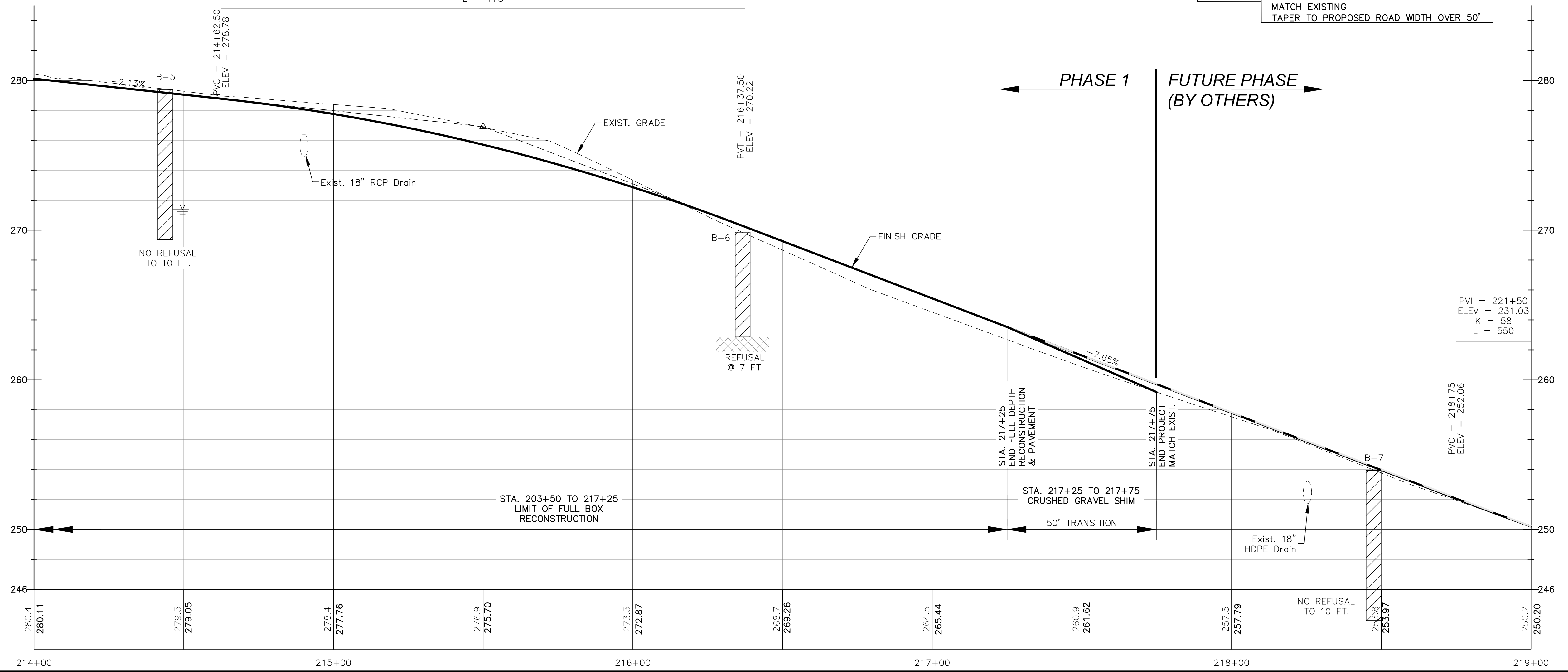
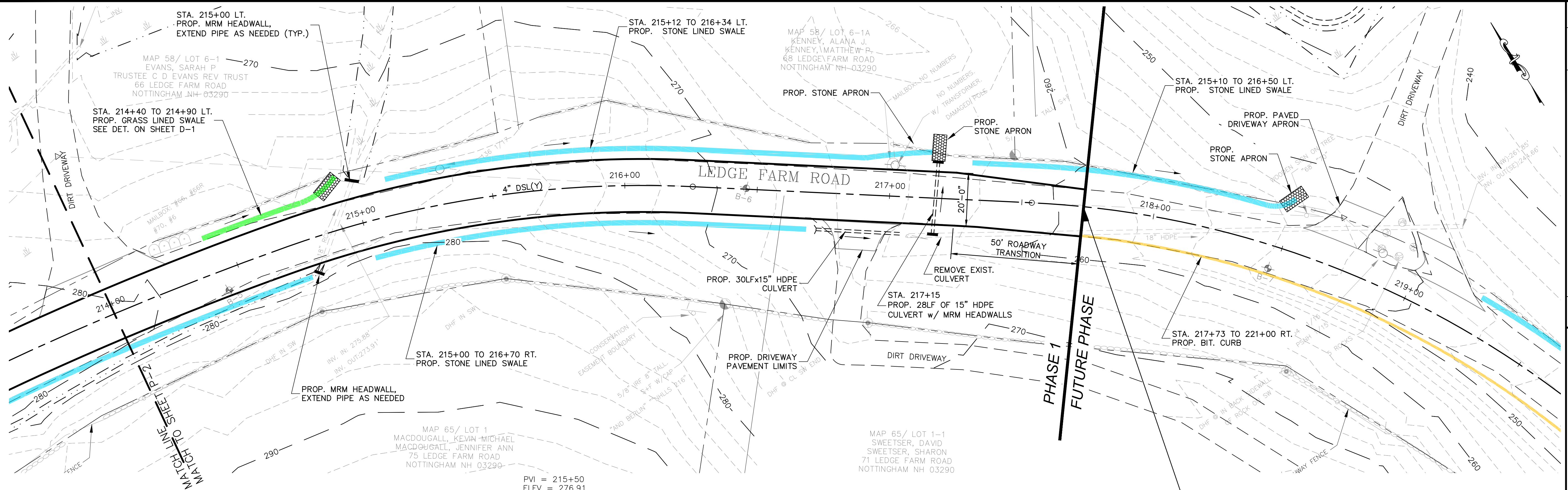
- Proposed Bituminous Curb
- Proposed Stone Lined Swale
- Proposed Grass Lined Swale
- Existing ROW / Property Line



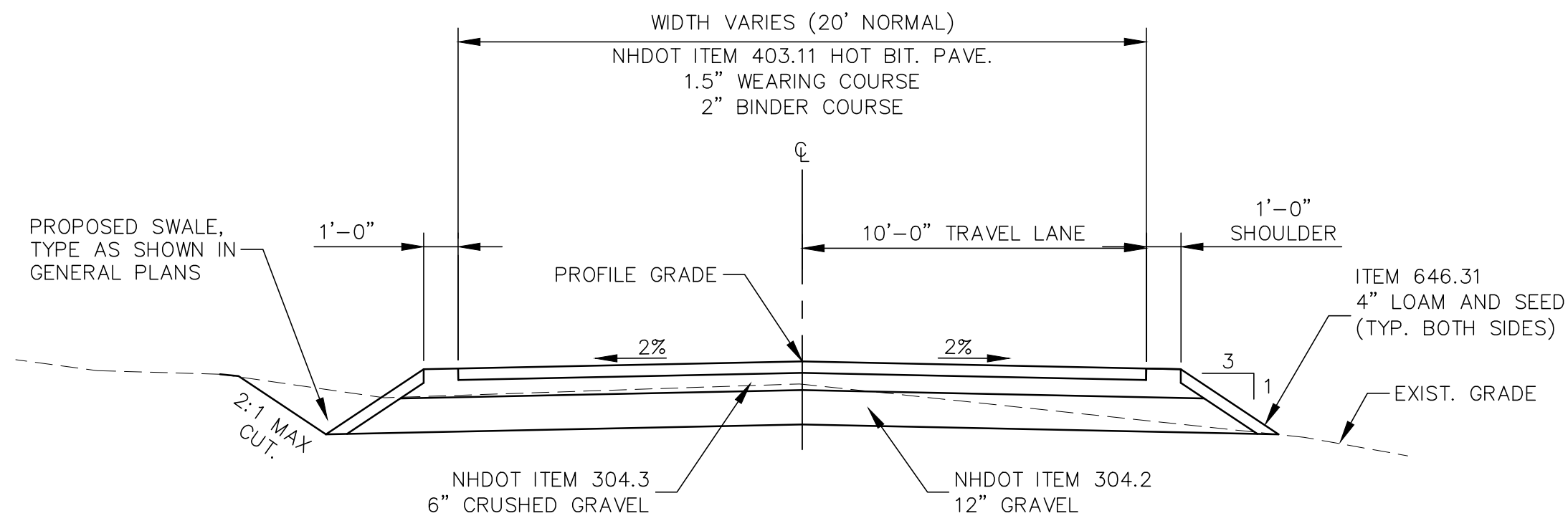
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DRAFT PLANS NOT FOR CONSTRUCTION					
date:	July 2019	project no.:	887	file name:	887_LFR_PLAN & PROFILE.dwg
designed by:	JWB	drawn by:	AGL	approved by:	WAS
scale:	0 20' 40'		Scale: 1" = 20'		
Town of Nottingham, New Hampshire Highway Department Ledge Farm Road Improvements Ledge Farm Road Plan & Profile Sta. 203+50 to Sta. 209+00					
drawing no. P-1					
sheet:	---	of	---	no.	1
issued for:	Bidding	revision		date	07/19
by:	JWB				



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DRAFT PLANS NOT FOR CONSTRUCTION				
date: July 2019	designed by: JWB	project no: 887	drawn by: AGL	approved by: WAS
Town of Nottingham, New Hampshire Highway Department		Ledge Farm Road Improvements		
Ledge Farm Road Plan & Profile Sta. 209+00 to Sta. 214+00				
drawing no. P-2				
sheet: --- of ---				

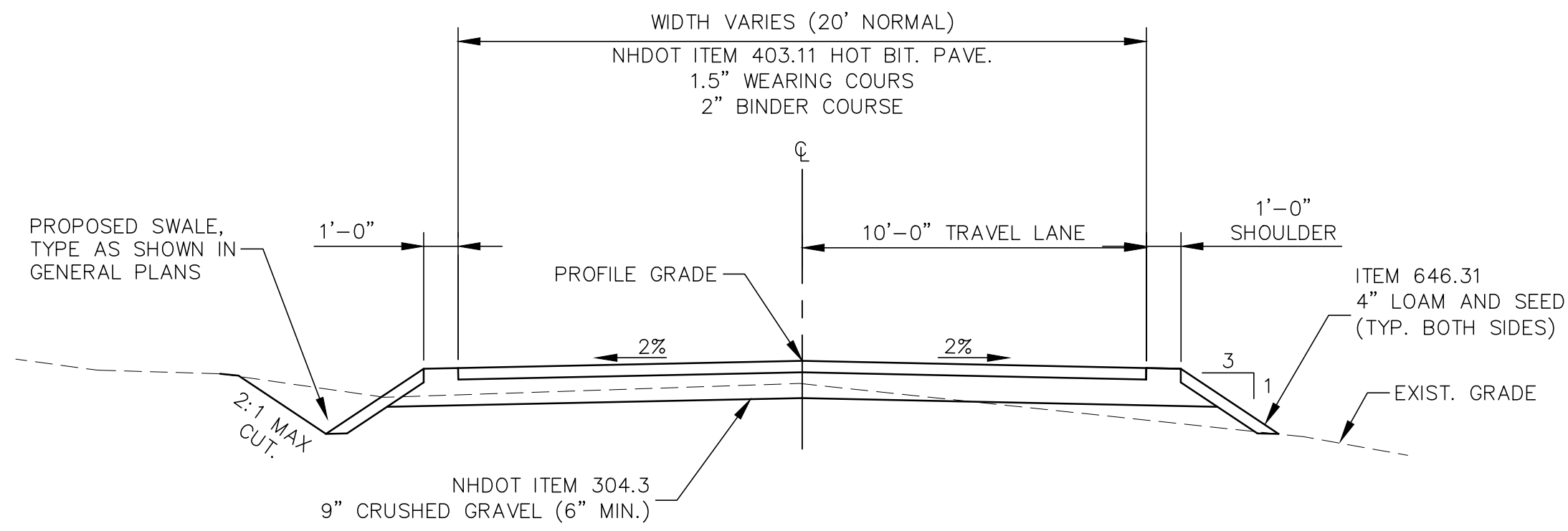


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DRAFT PLANS NOT FOR CONSTRUCTION		
date: July 2019 project no: 887 file name: 887_LFR_PLAN & PROFILE.dwg	designed by: JWB drawn by: AGL approved by: WAS	scale: 1" = 20' 0 20' 40'
Town of Nottingham, New Hampshire Highway Department Ledge Farm Road Improvements Ledge Farm Road Plan & Profile Sta. 214+00 to Sta. 219+00		
drawing no. P-3 sheet: --- of ---		



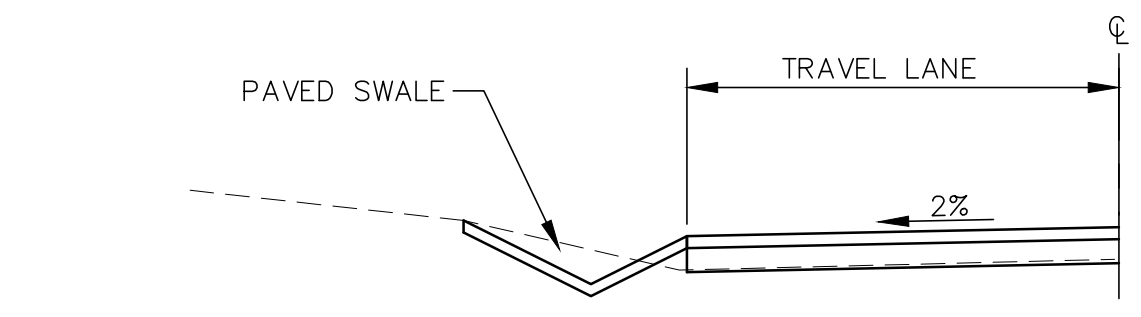
Paved Road Typical Section-Full Box Replacement

Scale: 1/4" = 1'-0"



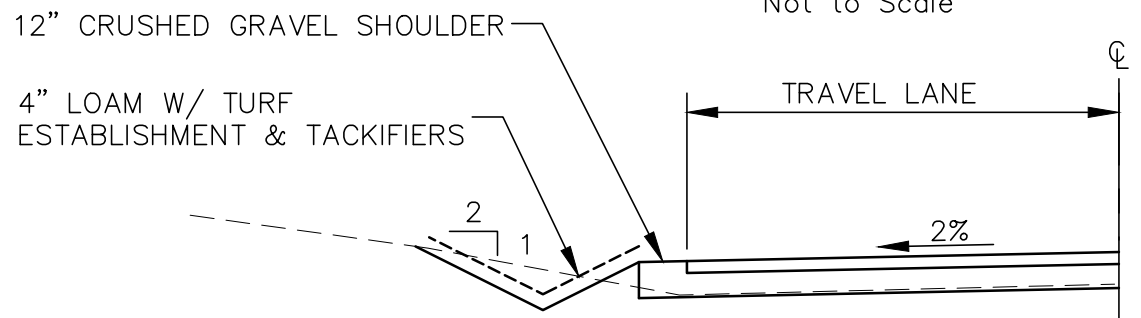
Paved Road Typical Section-Crushed Gravel Shim

Scale: 1/4" = 1'-0"



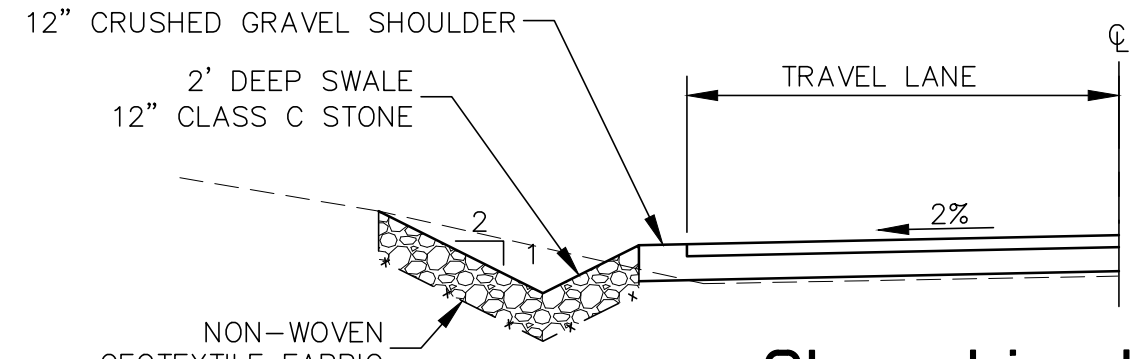
Paved Swale Detail

Not to Scale



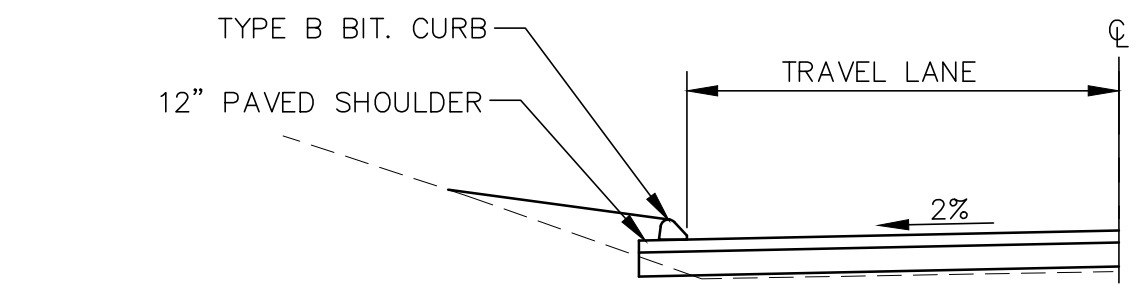
Grass-Lined Swale Detail

Not to Scale



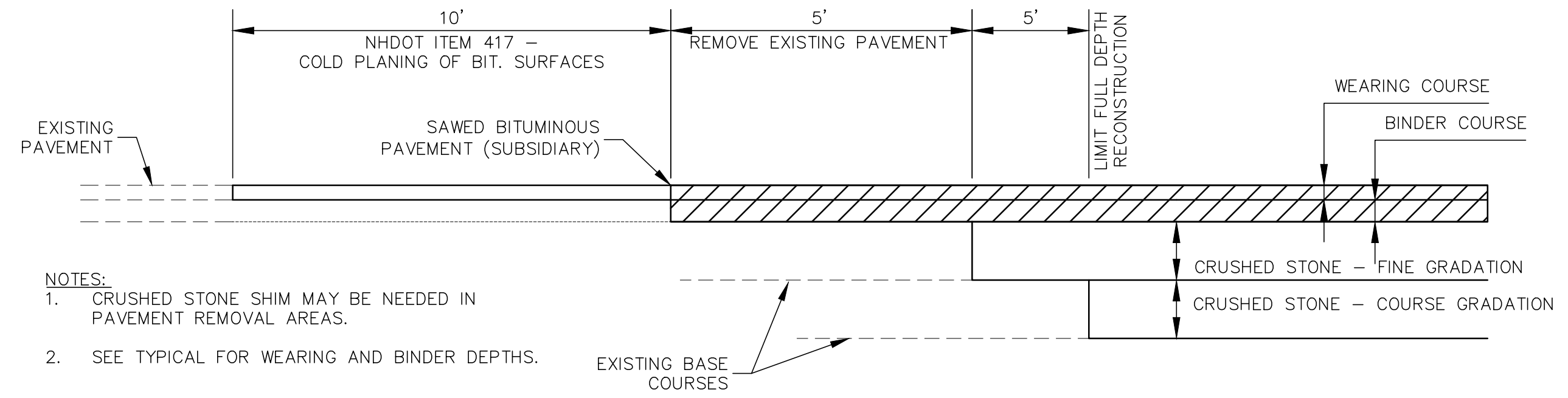
Stone-Lined Swale Detail

Not to Scale



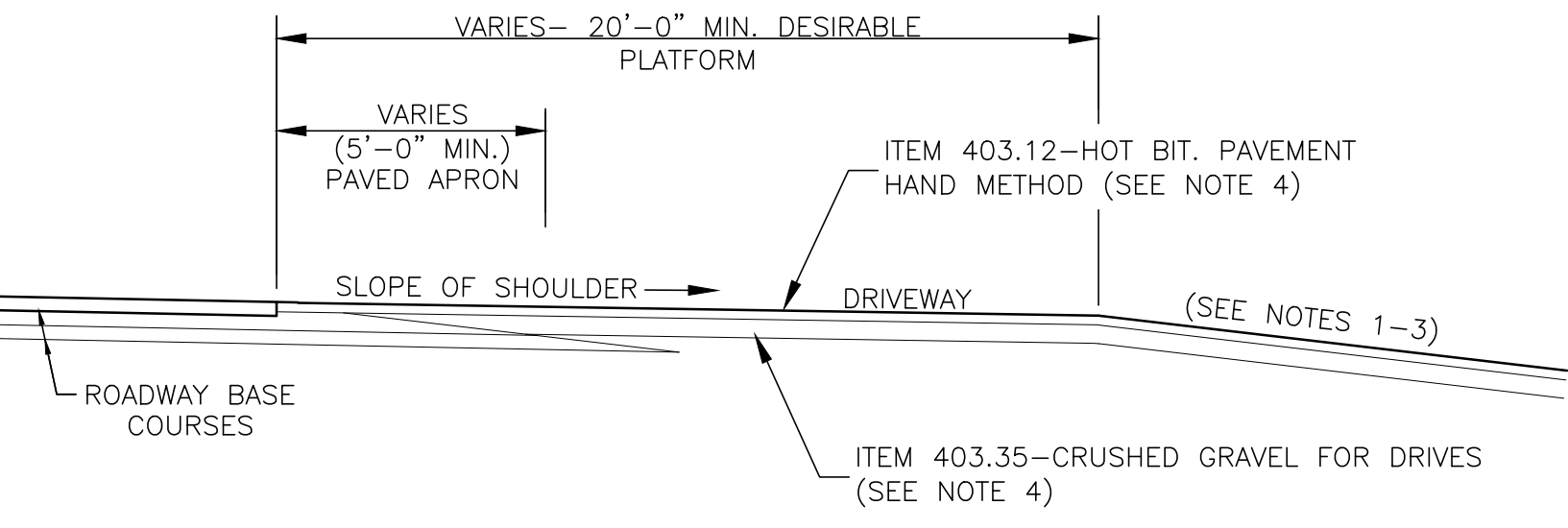
Bituminous Curb Detail

Not to Scale



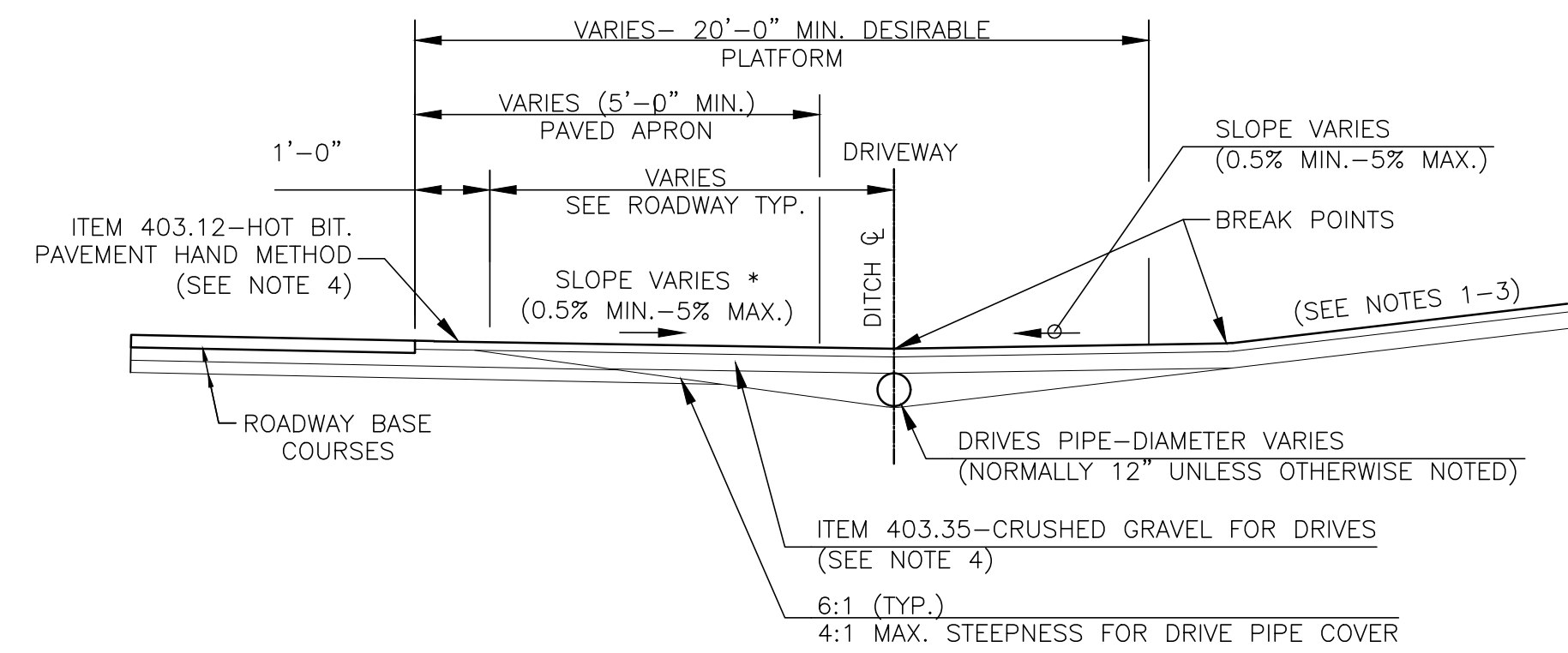
Pavement and Base Course Transitions

Not to Scale



Typical Driveway Section in Fill

Not to Scale



Typical Driveway Section in Cut

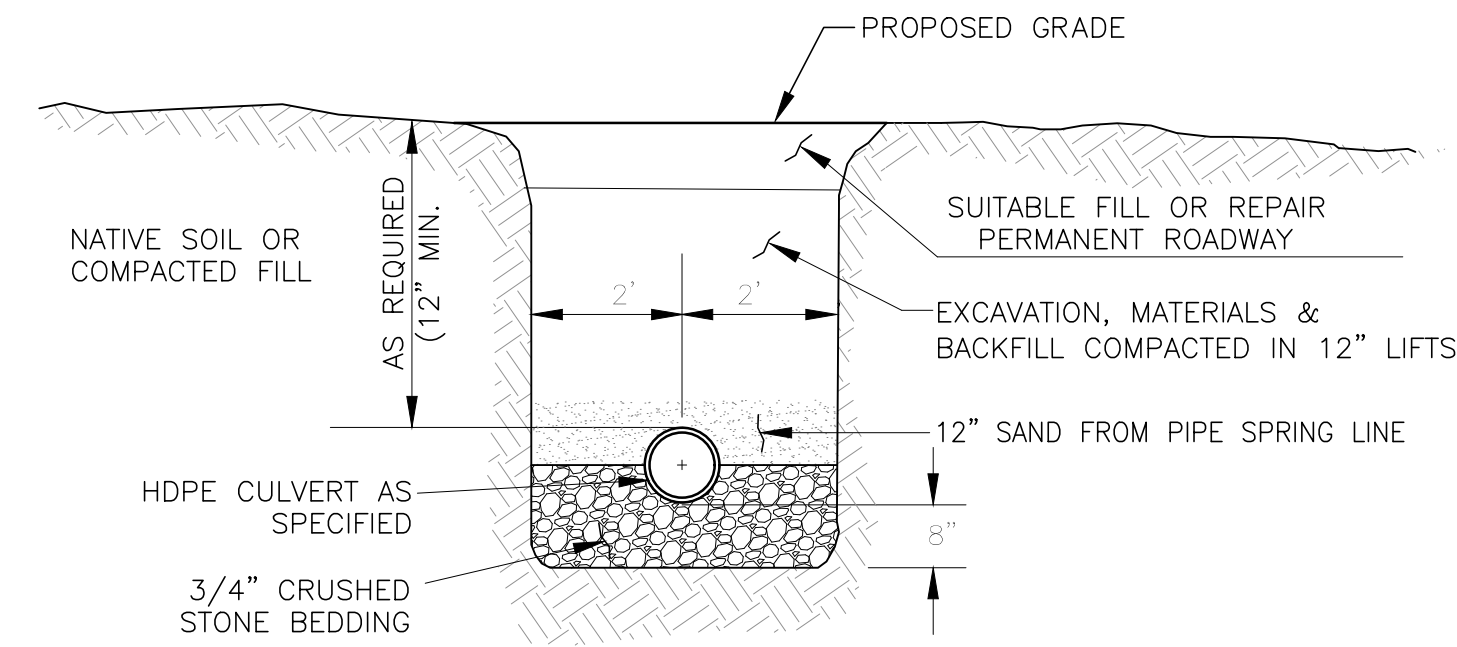
Not to Scale

- NOTES:**
- GRADES OF MAJOR ENTRANCES BEYOND THE PLATFORM SHOULD NOT EXCEED 8%.
 - GRADES OF OTHER DRIVES BEYOND THE PLATFORM SHOULD NOT EXCEED 15%.
 - THE ALGEBRAIC DIFFERENCE BETWEEN TWO ADJACENT GRADES SHOULD NOT EXCEED 10%.
 - PAVEMENT & BASE COURSE DEPTHS FOR RESIDENTIAL DRIVES SHALL BE 8" CRUSHED GRAVEL WITH 3" H.B.P. (HAND METHOD) PLACED IN TWO COURSES. IF THE DRIVE IS ADJACENT TO A ROADWAY WITH A CRUSHED STONE STRUCTURAL BOX, 8" OF CRUSHED STONE FINE GRADATION MAY BE SUBSTITUTED FOR THE 8" OF CRUSHED GRAVEL NOTED ABOVE.
 - PAVEMENT & BASE COURSE DEPTHS FOR COMMERCIAL DRIVES SHALL BE 12" CRUSHED GRAVEL WITH 3" H.B.P. (HAND METHOD) PLACED IN TWO COURSES. IF THE DRIVE IS ADJACENT TO A ROADWAY WITH A CRUSHED STONE STRUCTURAL BOX, 9" OF CRUSHED STONE FINE GRADATION MAY BE SUBSTITUTED FOR THE 12" OF CRUSHED GRAVEL NOTED ABOVE.
 - FOR UNPAVED DRIVES, THE PAVED APRON NORMALLY ENDS AT THE RADIUS TANGENT POINT.
 - DITCHES ARE RECOMMENDED FOR UNCURBED DRIVEWAYS IN CUT SLOPES.
 - THE CONTRACTOR SHALL LAYOUT PROPOSED DRIVEWAY RADIUS, FLARE, AND CURB TAPERS FOR REVIEW BY THE OWNER AND ENGINEER PRIOR TO CONSTRUCTION.

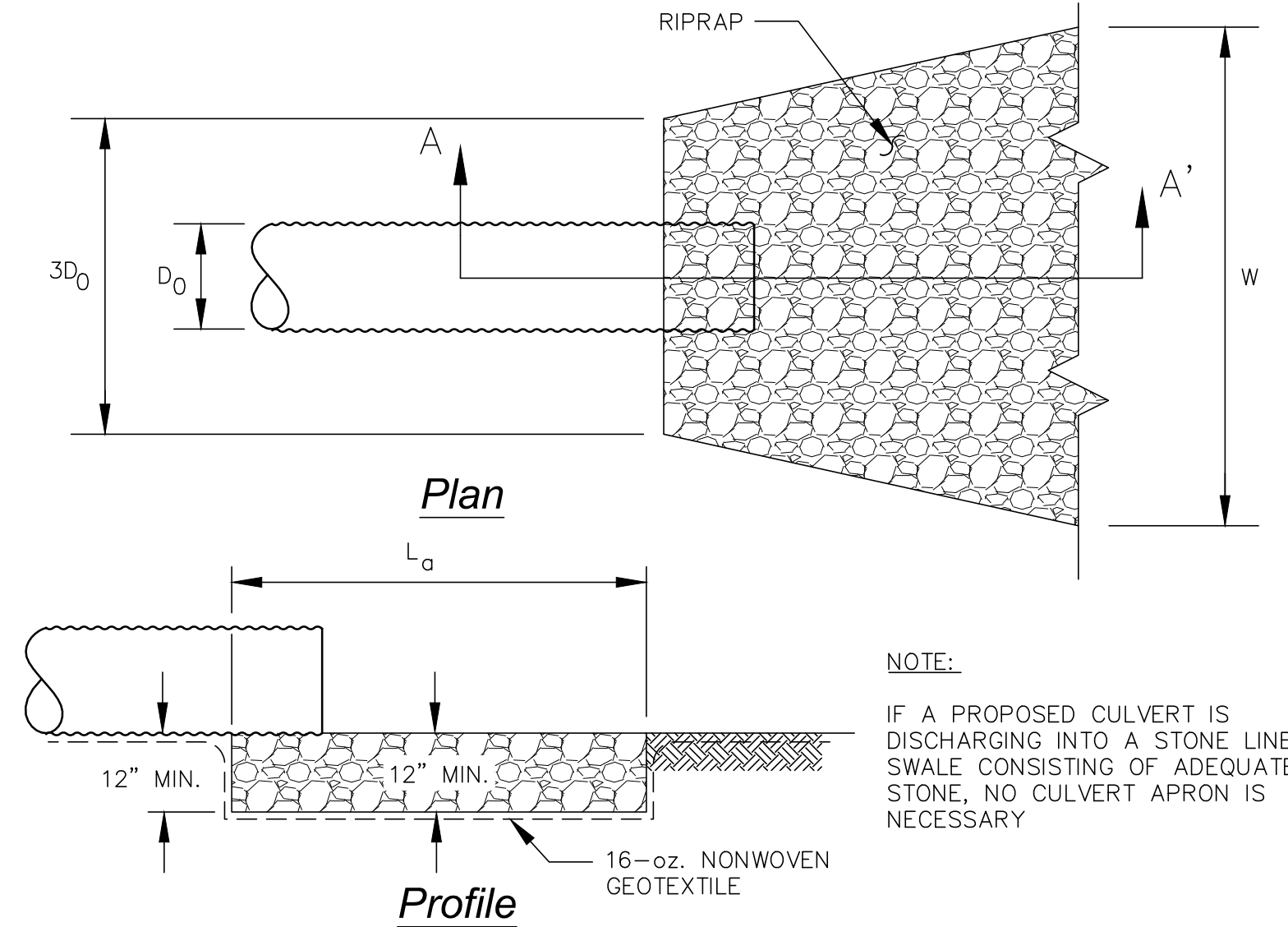
designed by:	JWB	drawn by:	AGL	approved by:	WAS	scale:		
date:	July 2019	project no.:	887	file name:	887_LFR_DETAILS.dwg			
<p>Town of Nottingham, New Hampshire Highway Department Ledge Farm Road Improvements Details</p>							drawing no.:	D-1
							sheet:	--- of ---
							revision:	1
							no.:	1
							issued for Bidding:	07/19
							date:	07/19
							by:	JWB

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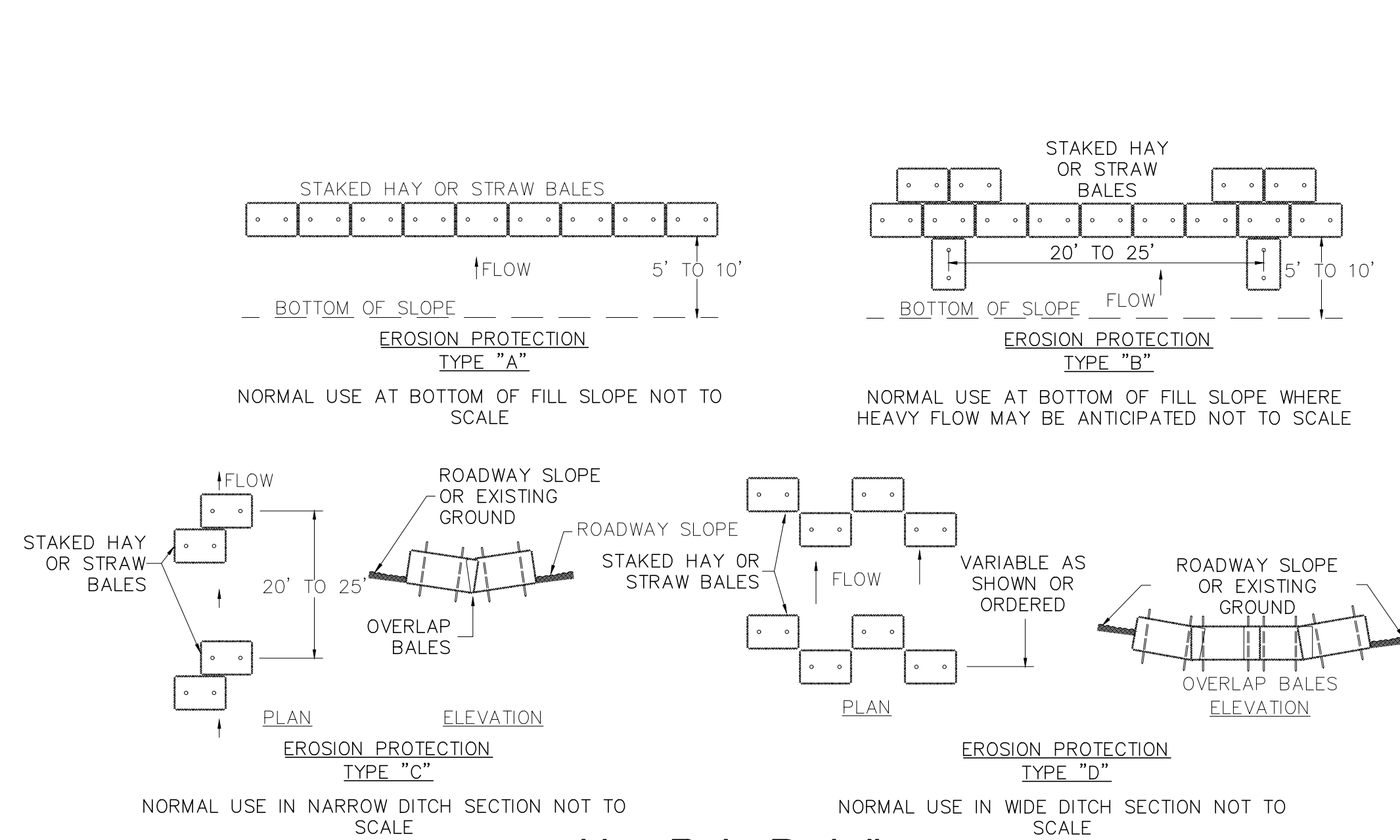


Culvert Installation Detail
NOT TO SCALE

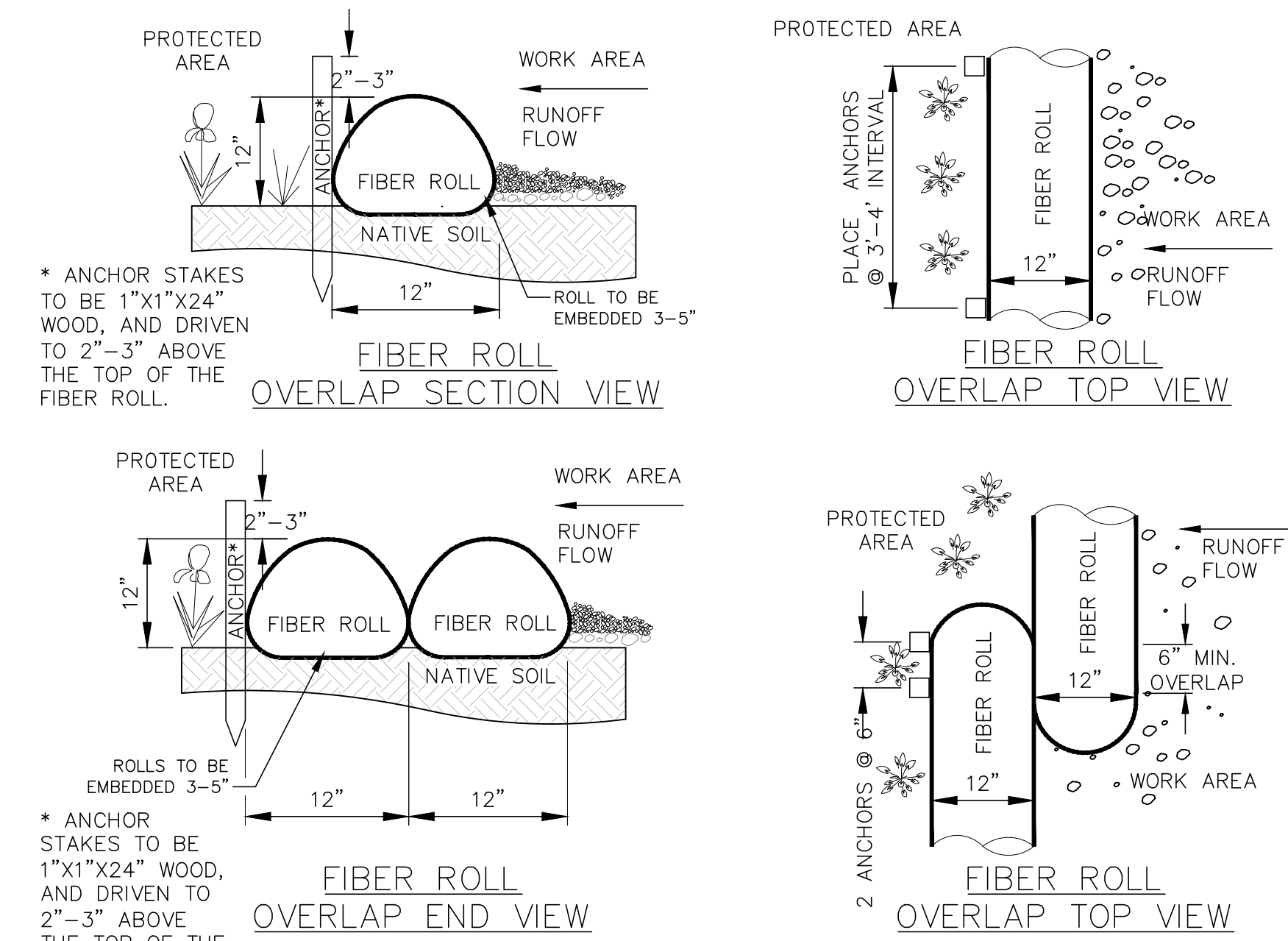


Stone Outlet Protection
Not to Scale

NOTE:
IF A PROPOSED CULVERT IS DISCHARGING INTO A STONE LINED SWALE CONSISTING OF ADEQUATE STONE, NO CULVERT APRON IS NECESSARY



Hay Bale Detail
Not to Scale



Erosion Control Fiber Roll
Not to Scale

no.	1	Issued for Bidding	date	07/19	by	JWB
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project no.:	887	drawn by:	AGL
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scale:			

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Highway Department
Ledge Farm Road
Improvements
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